

**CONNECTICUT RIVER BRIDGE  
019 MONROE – BARNET**

**NH Bridge ID: Monroe 110/125**

**CARRYING:** Barnet Road  
**PRESENT NAME:** Barnet Road Bridge  
**DATE BUILT:** 1937  
**LAT/LONG:** 44.287033,-72.057137



**CROSSING CHRONOLOGY**

- 1793 Stevens Ferry established
- 1812 First toll bridge built
- c.1829 Second bridge constructed
- 1850 Third bridge built
- 1852 Bridge destroyed; fourth bridge built
- 1854 Bridge wrecked by windstorm
- 1855 Fifth bridge constructed
- 1866 Bridge blown down
- 1875 Sixth span, a Town truss covered bridge, built
- 1923 Bridge closed due to unsafe condition
- 1924 Reopened as free bridge
- 1938 Seventh bridge, a steel Parker truss structure, completed
- 2003 Bridge rehabbed by Vermont



FIGURE 1: Barnet Road Bridge, built 1938. Downstream side from Vermont shore (Historic Documentation Company Inc. 2012).

## CROSSING HISTORY

**First crossing:** This span, known as the Barnet Bridge, and also as Stevens Bridge or Beards Falls Bridge, was constructed at a site located just below the crossing place for the Stevens Ferry, which had been established by Enos Stevens in 1793. The original bridge, constructed in 1812 by the proprietors of the Stevens Bridge Company, is referred to by local historian Frances Ann Johnson as having been a “log bridge.” Johnson speculates that this structure collapsed in the great flood of 1826.

**Second crossing:** At the end of 1828, the state legislature empowered “Calvin Palmer and his associates” to erect a new bridge.

**Third crossing:** Johnson states that a new bridge was built in 1850, although no explanation for the construction is given. On May 10, 1852, an ice and log jam carried away the bridge.

**Fourth crossing:** The bridge company immediately undertook to build a replacement structure, completing it in the autumn of 1852. A heavy gale demolished this span on September 6, 1854.

**Fifth crossing:** The next bridge built by the investors served for a longer lifespan. A powerful windstorm destroyed it as well, however, on May 4, 1866. For several years the bridge partners held off before making the investment in a new span.

**Sixth crossing:** In 1875, the Stevens Village Bridge Company employed Henry Paddleford to oversee construction of a substantial covered bridge. This structure extended 209 feet in length and was said to be the longest single-span wooden covered bridge in the world. The bridge was of Town truss design based on a wooden lattice framework. The latticework both supported the bridge’s roof structure and provided screen side walls for the bridge housing. At some date the company decided to reinforce the bridge with mammoth arches on either side that extended the full length of the span. The bridge continued as a privately owned toll bridge up to 1923, when the authorities condemned it as unsafe and it was closed. The partners sold the bridge to the Town of Monroe, with the town of Barnet, Vermont providing one third of the purchase price. The Town of Monroe had the bridge repaired and reopened it as a free crossing in 1924. In 1937, the NH Highway Department determined that the covered bridge should be replaced, apparently due to deteriorated condition and modern traffic demands.

**Seventh crossing:** During 1937-1938, the town had the contractor O.W. Miller Company, Inc., erect a new bridge. The single-span steel Pratt truss superstructure, 261 feet in length, was fabricated by the Lackawanna Steel Construction Corporation. The design of the overall new bridge included an approach span on the New Hampshire end consisting of a 117-foot steel pony Pratt truss that carried the roadway over a railroad line. The overall bridge was supported by a reinforced concrete substructure comprising abutments and a pier sited where the two trusses meet. This historic steel span remains intact and in use today, having undergone rehabilitation in 2003 by the State of Vermont.

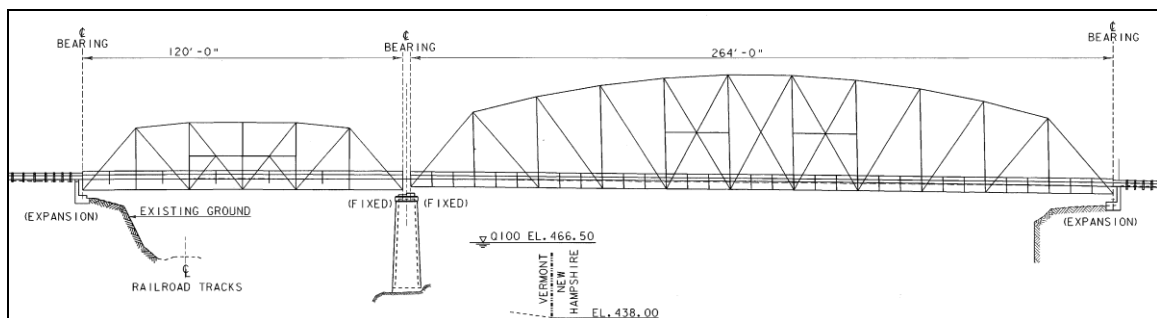


FIGURE 2: Elevation drawing from bridge rehabilitation plans (NHDOT 2000).



FIGURE 3: Stevens Village Bridge, built 1875, replaced 1937. Downstream side from Vermont, June 23, 1921 (Storrs).



FIGURE 4: Stevens Village Bridge, built 1875. New Hampshire portal, October 31, 1922 (Storrs).

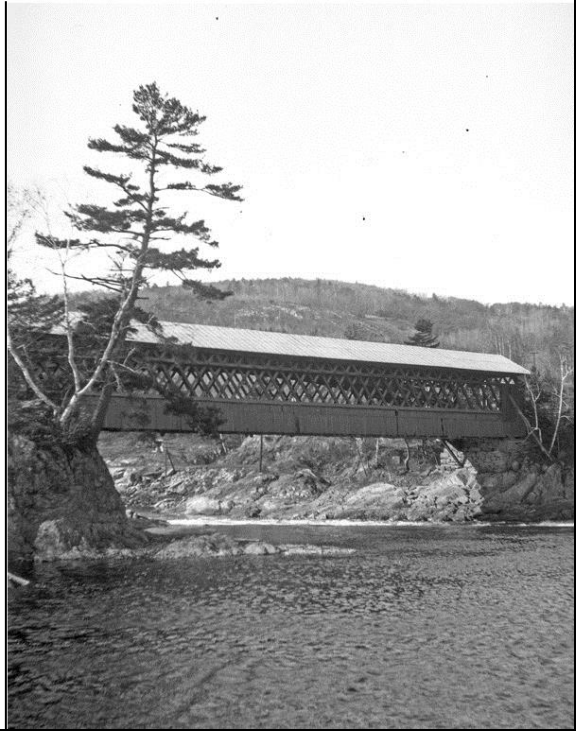


FIGURE 5: Stevens Village Bridge, built 1875. View of upstream side from Vermont, October 31, 1922 (Storrs).



FIGURE 6: Barnet Road Bridge, built 1938. Vermont end in 1941 (NHDOT 1941).



FIGURE 7: Barnet Road Bridge. Downstream side from Vermont in 1941 (NHDOT 1941).

## BIBLIOGRAPHY

- Johnson, Frances A. *The History of Monroe, New Hampshire, 1761-1954*. Littleton: Courier Printing Co., 1955.
- Knoblock, Glenn A. *New Hampshire Covered Bridges*. Charleston, SC: Arcadia Publishing, 2002.
- John Storrs Photograph Collection. Located at New Hampshire Division of Historical Resources, Concord.
- NHDOT Bridge Card, Monroe 110/125, 1937 and 1941. On file at NHDOT, Concord.
- NHDOT Bridge Drawings, Monroe 110/125, Feb. 2000. File Location No. 2-1-3-8. Filed at NHDOT, Concord.
- Report of the Bridge Commissioners of the State of New Hampshire. Manchester: State of New Hampshire, 1906.