

**CONNECTICUT RIVER BRIDGE
022 DALTON-LUNENBURG**

NH Bridge ID: Dalton 089/130
BYPASSED NH Bridge ID: Dalton 090/130

CARRYING: Bridge Hill Road
PRESENT NAME: varies, no formal name
DATE BUILT: 1997
LAT/LONG: 44.411191,-71.723235

CROSSING CHRONOLOGY

- 1792 Ferry established
- 1832 First bridge built
- c.1845 Bridge destroyed in spring freshet;
ferry resumed service
- 1928 Second bridge, steel deck truss
- 1933 Ferry discontinued
- 1997 Third bridge, steel continuous multi-beam
deck bridge built upstream of 1928 truss

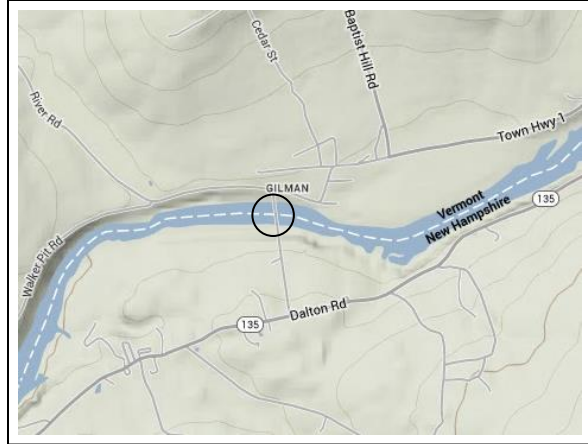


FIGURE 1: Dalton-Lunenburg bridges, 1997 deck girder bridge in foreground, 1928 deck truss bridge in background (Historic Documentation Co. Inc. 2012).

CROSSING HISTORY

Ferry Crossing: In 1792, early settler Moses Blake obtained a charter from the New Hampshire legislature to operate a ferry to Lunenburg, stationed at Blake's Tavern.

First bridge: Local lumbermen James B. Sumner and David H. Sumner acquired the Blake crossing right by 1828, when they petitioned the legislature to obtain a charter to build a toll bridge at this location. The Sumners constructed their timber bridge in 1831. As stated by local historian George Drew Merrill, "The bridge was built, and stood about thirteen years, and was carried off by the ice and logs in the spring of the year." The Sumners decided not to rebuild the bridge, instead reverting to ferry service. Merrill noted that the ferry was continuing when he published in 1888, and Connecticut River historian Charles Whittlesey was told by local informants that the ferry persevered on duty until around 1933. The ferry went on in its last years although a bridge was built nearby in 1928.

Second bridge: In 1928, the New Hampshire Highway Department constructed a highway bridge that still stands, although it has been a pedestrian-only structure since 1997. The bridge, 547 feet in length, is a three-span steel Pratt truss bridge carried on end piers and river piers built of reinforced concrete. The Berlin Bridge Company fabricated the steel structure. The bridge was extensively overhauled in 1994, but within a few years it was determined that it was unequal to current traffic demands and needed to be replaced. The bridge is currently barricaded. The truss bridge has been known as the Gilman Bridge, after the village of Gilman at the Vermont end of the bridge, and the Whitcomb Bridge, after E.E. Whitcomb, a Dalton Selectman in 1928 who was instrumental in getting the bridge built.

Third bridge: The new bridge was completed in 1997. It is a three-span continuous multiple beam deck bridge with five variable-depth welded plate girders and 591 feet in length overall.



FIGURE 2: Sumners' Ferry, originally established as Blake's Ferry, shown about 1925 (Dalton Historical Society).



FIGURE 3: Whitcomb Bridge, built 1928. Upstream side, view to Vermont in 1941 (NHDOT 1941).



FIGURE 4: Whitcomb Bridge. Downstream side, view to New Hampshire in 1941 (NHDOT 1941).



FIGURE 5: Whitcomb Bridge. Downstream side, view to Vermont (Historic Documentation Co. Inc. 2012).



FIGURE 6: Dalton-Lunenburg bridges, 1997 deck girder bridge, left, 1928 deck truss bridge, right (Historic Documentation Co. Inc. 2012).

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